Other Documents

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PLANNING SERVICES APPLICATION

CONSULTATION SHEET

APPLICANT:

Rolfe Judd Planning, Old Church Court, Claylands Road, The Oval, London, SW8 1NZ

APPLICATION NO: PP/99/01716

APPLICATION DATED: 01/08/1999

DATE ACKNOWLEDGED: 16 August 1999

APPLICATION COMPLETE: 16/08/1999

DATE TO BE DECIDED BY: 11/10/1999

SITE:

94/96 Draycott Avenue, London, SW3 3AD

PROPOSAL: Use of part of the pavement outside the restaurant for the placing of tables and chairs.

ADDRESSES TO BE CONSULTED 1. I JUES STREET, SW3 2. **3/5** 3. 🏂 5.48 DRAY COTT BURNUE, SW3+A 6. 100 A A 7.102+A 8. 139 9. The Principal 10. 11.

The Telephone Dicharge, to 94 Dray with Avenue, Sw3

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12.

13.

14.

15.

HBMC Listed Buildings HBMC Setting of Buildings Grade I or II HBMC Demolition in Conservation Area Demolition Bodies DoT Trunk Road - Increased traffic DoT Westwayetc., Neighbouring Local Authority Strategic view authorities Kensington Palace Civil Aviation Authority (over 300')

Theatres Trust

National Rivers Authority

CONSULT STATUTORILY

Thames Water Crossrail

LRT/Chelsea-Hackney Line

ADVERTISE

Effect on CA

Setting of Listed/Building

Works to Listed/Building Departure from UDP

Demolition in CA

"Major Development"

Environmental Assessment

X No Site Notice Required

Notice Required other reason Police

L.P.A.C

British Waterways

Environmental Health

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EVELOPME I CONT THE ROYAL TECHNICAL BOROUGH OF PP991716 **KENSINGTON** AND CHELSEA **POLLING DISTRICT** HB **Buildings of Architectural Interest** Local Shopping Centre LSC **AMI** Areas of Metropolitan Importance ΑI Sites of Archeological Importance MDO Major Sites with Development Opportunities SV Designated View of St Paul's from Richmond MOL Metropolitan Open Land SNCI Sites of Nature Conservation Importance SBA Small Business Area REG 7 Restricted size and use of Estate Agent Boards **PSC** Principal Shopping Centre (Core or Non-core) ART IV Restrictions of Permitted Development Rights Conservation HB TPO AMI MDO MOL SBA Unsuitable for PSC SNCI REG 7 ART IV Area Diplomatic use Ν Within the line of Safeguarding of the proposed Chelsea/Hackney underground line Within the line of Safeguarding of the proposed Eastwest/Crossrail underground line Density Notes: Site Area Habitable rooms proposed Proposed Density Plot Ratio Site Area Zoned Ratio Floor Area proposed Proposed Plot Ratio Complies **Daylighting** Infringes Spaces required **Car Parking** Spaces proposed

THE ROYAL **BOROUGH OF**

NOTICE OF A PLANNING APPLICATI

TOWN AND COUNTRY PLANNING ACT 1990 PLANNING (LISTED BUILDINGS AND CONSERVATION AREAS) ACT 1990



KENSINGTON AND CHELSEA

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MEMORANDUM

TO: FOR FILE USE ONLY

EXECUTIVE DIRECTOR From:

PLANNING & CONSERVATION

My Ref: __PP/99/01716/AJF _____ CODE- SL

CARROLL STORY CONTRACTOR STORY PANAME TO BELLEVIEW

Room No:

Date:

23 August 1999

DEVELOPMENT AT:

94/96 Draycott Avenue, London, SW3 3AD

DEVELOPMENT:

Use of part of the pavement outside the restaurant for the placing of tables and chairs.

The above development is to be advertised under:-

NO REQUIREMENT FOR SITE NOTICE/ADVERTISEMENT IN THIS CASE.

M.J. French

Executive Director, Planning & ConservationSITE NOTICE CRITERIA NOT MATCHED **CHECK CORRECT CODE IS ENTERED**

PLANNING AND CONSERVATION

THE TOWN HALL HORNTON STREET LONDON W8 7NX

Executive Director M J FRENCH FRICS TP MRTPI Cent TS

Switchboard

Switchboard: 0171-937-5464

Extension: 326

Direct Line: 0171-361- 3260

KENSINGTON AND CHELSEA

THE ROYAL BOROUGH OF

Facsimile:

0171-361-3463

Date: 23 August 1999

My reference:

Your reference:

Please ask for:

My Ref: DPS/DCSE/PP/99/01716/AJF

Planning Information Office

Dear Sir/Madam,

The Occupier

1 file copy

file copy

file copy

TOWN AND COUNTRY PLANNING ACT 1990

Proposed development at: 94/96 Draycott Avenue, London, SW3 3AD

Brief details of the proposed development are set out below. Members of the public may inspect copies of the application, the plans and any other documents submitted with it. The Council's Planning Services Committee, in considering the proposal, welcomes comments either for or against the scheme. Anyone who wishes to make representations about the application should write to the Council at the above address within 21 days of the date of this letter. Unfortunately, the Council does not have the resources to advise objectors of the Committee date, and you should telephone for further information.

Proposal for which permission is sought

Use of part of the pavement outside the restaurant for the placing of tables and chairs.

Applicant

Rolfe Judd Planning, Old Church Court, Claylands Road, The Oval, London, SW8 1NZ

PLEASE NOTE: Applications for districts W.10, W.11 & W.2 in the NORTH of the Borough can be seen at: THE INFORMATION CENTRE, NORTH KENSINGTON LIBRARY, 108 LADBROKE GROVE, W.11 (Tel: 0171 727 6583) and NOT at the location stated overleaf.

Yours faithfully,

M. J. FRENCH

Executive Director, Planning and Conservation



WHAT MATTERS CAN BE TAKEN INTO ACCOUNT

When dealing with a planning application the Council has to consider the policies of the Borough Plan, known as the Unitary Development Plan, and any other material considerations. The most common of these include (not necessarily in order of importance):

- * The scale and appearance of the proposal and impact upon the surrounding area or adjoining neighbours;
- * Effect upon the character or appearance of a Conservation Area;
- * Effect upon the special historic interest of a Listed Building, or its setting;
- Effect upon traffic, access, and parking;
- * Amenity issues such as loss of Sunlight or daylight, Overlooking and loss of privacy, Noise and disturbance resulting from a use, Hours of operation

WHAT MATTERS CANNOT BE TAKEN INTO ACCOUNT

Often people may wish to object on grounds that, unfortunately, <u>cannot</u> be taken into account because they are not controlled by Planning Legislation. These include (again not in any order of importance):

- Loss of property value;
- * Private issues between neighbours such as land covenants, party walls, land and boundary disputes, damage to property;
- * Problems associated with construction such as noise, dust, or vehicles (If you experience these problems Environmental Services have some control and you should contact them direct)
- * Smells (Also covered by Environmental Services)
- Competition between firms;
- * Structural and fire precaution concerns; (These are Building Control matters)

WHAT HAPPENS TO YOUR LETTER

Planning applications where objections have been received are presented to the Planning Services Committee which is made up of elected Ward Councillors. Planning Officers write a report to the Committee with a recommendation as to whether the application should be granted or refused. Letters received are summarised in the report, and copies can be seen by Councillors and members of the public including the applicant. The Councillors make the decisions and are not bound by the Planning Officer's recommendation. All meetings of the Committee are open to the public.

If you would like further information, about the application itself or when it is likely to be decided, please contact the Planning Department on the telephone number overleaf.

WHERE TO SEE THE PLANS

Details of the application can be seen at the Planning Information Office, 3rd floor, Town Hall, Hornton Street W.S. It is open from 9am to 4.45pm Mondays to Thursdays (4pm Fridays). A Planning Officer will always be there to assist you.

In addition, copies of applications in the Chelsea Area (SW1, SW3, SW10) can be seen at The information Office, Chelsea Old Town Hall, Kings Road SW3 (0171-352 1856), for the Central Area (W8, W14, SW5, SW7) can be viewed in the Central Library, Town Hall, Hornton Street, W.8. and applications for districts W10, W11 and W2 in the North of the Borough can be seen at The Information Centre, North Kensington Library, 108 Ladbroke Grove, London W11 (under the Westway near Ladbroke Grove Station 0171-727-6583). Please telephone to check the opening times of these offices.

If you are unable to come to the Office due to illness or disability, it may be possible for a Officer to come to your home with the plans. Please contact the Planning Department and ask to speak to the Case Officer for the application.

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	RUBK&C TRAN	SPORTATIOR COMMENTS	· · · · · · · · · · · · · · · · · · ·	
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PP Number	Address		Date of obs	
99/1716	94-96 Draycott Avenue		26 Aug 1999	
Proposal:			Obj	No Obj
-	-	the restaurant for the placing of	$ \sqrt{} $	
tables and chair	S.		•	
File Number	Obs	Transportation Officer	D C Of	l ficer
TF/202/D	INITIAL	Steve Lauder	AJF	
Other informe	tions	·	•	

Other information:

COMMENTS:

With reference to the above application, I have the following comments:

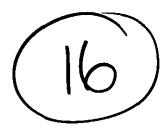
I donot concur with the applicants contention that part of the footway is private forecourt. So far as I am aware the footway at this location is all maintained at public expense. The responsibility for the maintenance of the cellar doors lies with the owner or occupier of the premises, as specified in section 180 of the Highways Act 1980. This however has no bearing on the status of this area of land.

With regard to the proposed plans, I have concerns that whilst the applicants have shown the tables and chairs as being positioned so as to allow 1.2 metres of footway for pedestrians, in practical terms there is likely to be a 'sprawl effect' which may lead to the entire footway being obstructed.

In order to reduce the risk of this happening, I would request that the applicants are invited to submit a revised proposal containing the same number of tables, however limiting each table to two chairs, and placing all of the tables flush to the wall.

The applicants should also be reminded at this stage that should they be granted planning permission, additional consent will also be required under the terms of the Highway Act 1980 for use of the public highway.

Signed TOM MANSFIELD



PLANNING ARCHITECTURE INTERIORS

Old Church Court

Claylands Road The Oval London SW8 INZ Telephone: 0171 556 1500 Fax: 0171 556 1501

E-Mail: jeffp@rolfe-judd.co.uk

JGP/P1087 3 September 1999

Director

Planning & Conservation

The Royal Borough of Kensington & Chelsea

The Town Hall

Hornton Street

London W8 7NX.

For the attention of Alison Flight

Direct Dial Telephone: 0171 556 1617

EXAMPLE SERVICES

N C SW SP ENF AO ACK

0 6 SEP 1999

EC ARB FWD CON PLES

Dear Sirs

Town & Country Planning Act 1990 Application for Planning Permission (RN: PP/99/01716)

Brompton Bay Restaurant, 94-96 Draycott Avenue, London, SW3

Following discussion by telephone on the 1st September 1999 we enclose four copies of an amended drawing showing proposed pavement dining detailed as follows:

n to Ea.

DescriptionScaleDrawing NumberCopiesProposed pavement dining1:50A3/3408/SK101/14

In the light of comments received from the Highways Department, tables and chairs are now only proposed where their positioning will leave at least the minimum 1800mm width unobstructed highway. We have also placed the tables flush against the walls of the restaurant in order to prevent the spread of customers into the pavement.

The application now proposes the positioning of tables and chairs on the pavement as follows:

- Two tables with two chairs each would be placed to the west of the entrance on Draycott Avenue.
- A table and three chairs would be placed at the corner.

Continued/...



Director Planning & Conservation 3 September 1999/ P1087

We trust this meets yours and the Highways Department's requirements and that you now have sufficient information to determine the application. Please do not hesitate to call if you wish to discuss matters.

Yours sincerely

Jeff Pyrah MRTPI Rolfe Judd Planning

encl.

cc + encl.:

Mr B Shirazi

Zagros Corporation Ltd

	RBK&C TRA	NSPORTATION COMMENTS:		er an partie als ands said als a
PP Number	Address		Date of obs	
99/1716	94-96 Draycott Avenue		14 Sept 1999	
Proposal: Use of part of p tables and chair		e restaurant for the placing of	Obj	No Obj
File Number TF/202/D	Obs FULL	Transportation Officer Steve Lauder	D C Officer AJF	

Other information:

To be read in conjunction with previous observations dated 26 August 1999.

COMMENTS:

With reference to the above application, I have the following comments:

A revised drawing showing 2 tables with 2 chairs, and 1 table with 3 chairs has been submitted for consideration.

The drawing indicates that the positioning of these tables and chairs will leave a minimum of 1.8 metres of footway unobstructed. This is reiterated in Rolfe Judd's letter of 3 September 1999.

My previous objection is now withdrawn, subject to the applicants continued compliance with their undertaking to leave a minimum 1.8 metres of footway unobstructed.

igned OF MANSFIELD

Royal Borough of Kensington and Chelsea Directorate of Planning Services - Access Observations



ACCESS: TP/99/1716	Address: 94-94 Draycott Avenue	Date Rec 22/09/9		Date of Obs. 27/09/99
UDP Paras/Policies		Obj.		No obj. ✓
	Development: Use of part of the pavement outside the restaurant for the placing of tables and chairs.	НМО?	No. of Existing	Dwelling Units Proposed
		D.C. Offi AJF	cer	Policy Officer AKS

Access Comments:

Paragraph 5.3.20 (a)of the UDP states:

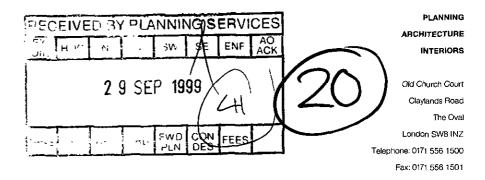
All 'high use' routes must be at least 1800mm wide to allow people with prams and wheelchairs to pass without difficulty. Other routes must not be less than 1200mm wide.

Drawing Number A3/3408/SK 101 shows a clearance of 1.8m on most of the pavement and greater than 1.2m on the remainder. This is not a 'high use' route.

With the above in mind I have no objection to this application. However the applicant should be made aware that a degree of 'sprawling' tends to occur in situations such as this should be controlled by the staff of the restaurant.

No Objection.	
Monitoring	<u> </u>
If the comments above are agreed I have no wish to be kept abreast of this application.	
I wish to kept abreast of any recommendations made on this application prior to submission of the report.	
I wish to be actively involved in discussions on this application.	

RG 28/9/99



JWK/JGP/P1087 28 September 1999

By Fax and Post

Director
Planning & Conversation
The Royal Borough of Kensington & Chelsea
The Town Hall
Hornton Street
London W8 7NX

E-Mail: johnk@rolfe-judd.co.uk Direct Dial Telephone: 020 7556 1545

For the attention of Alison Flight





Dear Sirs

Town & Country Planning Act 1990
Application for Planning Permission (RN: PP/99/01716)
Brompton Bay Restaurant: 94-96 Draycott Avenue

Further to our telephone conversation today, we confirm that to deal with the concerns of adjoining occupiers, our client is prepared to install a temporary barrier when tables are chairs are placed on the pavement in order to delineate the area. This barrier will be placed at the western end of the designated area in order to delineate it and to prevent seating straying outside the area towards the access to 93 Draycott Avenue.

We understand that this proposal does not require planning permission. We trust that this addresses the concerns of the owner of 93 Draycott Avenue. We are forwarding this letter to Mr Collins by fax for his information. Please do not hesitate to call if you wish to discuss matters further.

Yours sincerely

John W Kerr MRTPI Rolfe Judd Planning

* procedure 1 to 1 to 1

Mr J Collins

93 Draycott Avenue

Ry: SP/ P1087

THE ROYAL BOROUGH OF KENSINGTON AND CHELSEA MEMORANDUM - SECTION 101 - LOCAL GOVERNMENT ACT, 1972

To:

Chief Administrative Officer (Planning)

From:

The Executive Director, Planning & Conservation

Date: 6 January 2000

Our Ref: PP/99/01716

Application Date: 01/08/1999 Complete Date: 16/08/1999 Revised Date: 06/09/1999

Agent:

Rolfe Judd Planning, Old Church Court, Claylands Road, The Oval, London,

SW8 1NZ

Address:

94/96 Draycott Avenue, London, SW3 3AD

Chelsia

This application is for a class of development to be determined under powers delegated to me by the Council on 21st July, 1976, and/or by the Town Planning Committee 6th November, 1979, and/or by the Town Planning Committee on 18th March 1986, 23rd May 1988 and 8th January 1990 and is not such as I wish to refer to the Planning Application Committee. No relevant planning objections have been received.

Class (i) - 8th Schedule development

Class (vi) - Listed building consent for above Classes.

Class (ii) - shop fronts

Class (vii) - approval of facing materials

Class (iii) - conversion from non s/c dwellings etc

Class (iv) - amendments as required LEGATED by T.P. Committee APPROVAL

Class (viii) - grant of planning permission for a change from one kind of non-residential use except from one kind of non-residential use except where this would involve the

loss of shop use

Class (v) - above classes after p.P. 18 16 2000 Committee agree

Class (ix) - grant permission licence or nd objection under Sections 73, 74, 138, 143, 152, 153, 177 & 180 of the Highways Act

Class (x) - Crossover under S. 108 of the Highways Act 1980

Consent under T&CP Control of Advertisement Regulations 1984-90; incl. refusal of consent for Reg. 15 applications.

DESCRIPTION OF PROPOSED DEVELOPMENT

The placing of three tables and seven chairs to the pavement for use in conjunction with the existing restaurant premises.

RECOMMENDED DECISION Grant planning permission conditionale RBK&C drawing(s) No. PP/99/01716 Applicant's drawing(s) No.RBKC Drawing Nos: PP/99/1716 & PP/99/1716 Rev A

Applicants Drawing Nos:

A3/3408/SK101, A3/3408/SK101/Rev 1

I hereby determine and grant/refuse this application (subject to HBMC Direction/Historic Building authorisation) under the powers delegated to me by the Council, subject to the condition(s) indicated below imposed for the reason(s) appearing thereunder, or for the reasons stated.

Exec. Di nning and Conservation

Head of Development Control

Area Planning Officer

11.1.00

PP/99/01716: 1

CONDITIONS AND REASONS FOR THEIR IMPOSITION



1. The development hereby permitted shall be begun before the expiration of five years from the date of this permission. (C001)

Reason As required by Section 91 of the Town and Country Planning Act 1990, to avoid the accumulation of unexercised Planning Permissions. (R001)

12%

Thereshal be maximum of three tables and seven chairs placed upon the pavement at any one time and these shall be placed in the positions indicated on Drawing No: A3/3408/SK 101/Rev 1

Reason: To safeguard the amenities of neighbouring properties

The tables and chairs hereby approved shall be removed from the pavement by 23.00 hours on any day and no part of the public highway shall be used for restaurant purposes after that time and of below 8.00 hours Reason: To safeguard the amenities of neighbouring properties

4. This permission shall be personal to Zagros Corporation Ltd and shall not enure for the benefit of the land. (C006)

Reason - In granting this permission the local planning authority has had regard to the special circumstances of the case, and considers that use otherwise than by Zagros Corporation Ltd. would have had different implications, and would have

to the special circumstances of the case, and considers that use otherwise than by Zagros Corporation Ltd. would have had different implications, and would be likely to have resulted in planning permission being refused. (R006)

5. The tables and chairs hereby permitted shall be retained for a limited period only until 31 January 2001 on or before which date the use shall be discontinued. (C004)

<u>Reason</u> - There is insufficient evidence available at this stage to assess the impact of the development, and permission for a limited period will allow the authority to reassess the development in the light of experience of the use. (R004)

INFORMATIVES

- 1. I10
- 2. 125
- 3. I21

DELEGATED REPORT

Address

94/96 Draycott avenue Sw3

Reference

Conservation Area

Listed Building Yes/No

of Development	
esidential Extension	Shopfront
Roof	Non-Residential Extension
Rear	
Side	Listed Building Demolition whole part
Front	· · · · · · · · · · · · · · · · · · ·
Garden	Listed Building Alterations
Residential Alterations	
· · · · · · · · · · · · · · · · · · ·	<u> </u>
Residential Conversion	Unlisted Building - Demolition
rom	whole part
Го	

/ho	-(9(L)
vercome by Amendment/Withdrawn/Not Relevant/Other	
xisting	
ssues/Policy/Precedent/Conditions/Third Schedule	Standards satisfactory
	Light
	Privacy
	Room Sizes
see attached	Parking
see atteched	Trees
	HBMC Direction/Obs.
	Obs. Rec'd
·	DirectionRec'd
	Consultation Expired
	·
	,
GRANT/APPROVE /	
	· ·

Date

Report by

BOF

Agreed

DELEGATED REPORT

PP/99/01716



1.0 **SITE**

- 1.1 The property is a four storey plus basement building situated at the end of a short terrace of four properties on the North side of Draycott Avenue at its junction with Ives Street. The property is currently in use as a restaurant
- 1.2 The property is not a listed building and neither is included within nor abuts any designated conservation area

2.0 PROPOSAL

2.1 The application is for the placing of two tables(each with two chairs) adjacent to No 98 Draycott Avenue and one table with three chairs at the junction of Draycott Avenue and Ives Street.

3.0 PLANNING CONSIDERATIONS

- 3.1 The main considerations in this case are the effect of tables and chairs upon the free flow of pedestrians along this part of the commercial frontage, together with their impact on the levels of residential amenity enjoyed by the occupants of nearby properties
- 3.2 The relevant planning policies for consideration are:

TR21 (pedestrian footways) CD34 (noise and disturbance)

- 3.3 With regard to pedestrian movement on the footway, the two tables adjacent to No 98 Draycott Avenue would occupy 3600mm of the frontage and leave a remaining pavement depth of 1800mm. The single table would occupy 1500mm of the frontage and leave a pavement depth of some 2000mm. The proposed location of the tables and chairs would result in a remaining pavement depth sufficient to allow the free flow of pedestrian movement and complies with this Councils' standards which states that 'high use' routes must be at least 1800mm wide to allow people with prams and wheelchairs to pass without difficulty. This Councils' Transportation Officer and Access Officer have stated they have no objection to the proposal. Therefore, the proposal is considered to comply with Policy TR21
- 3.4 With regard to possible noise and disturbance. Given that the premises front Draycott Avenue which is commercial in nature, in particular the property directly opposite 'La Belle Epoque' is a large restaurant premises which has planning permission for fourteen tables and twenty eight chairs to the pavement area. Therefore, the ambient noise level in terms of both pedestrian and vehicular activity is relatively high and continues into the late evening hours. In these circumstances, the placing of three tables and seven chairs to the pavement area is not considered to result in any increase in noise and

PP/99/01716: 3

disturbance that was so significant it would harm the existing levels of amenity currently enjoyed by occupiers of surrounding properties. Therefore, the proposal is considered to comply with Policy CD34

4.0 **PUBLIC CONSULTATION**

4.1 Eleven letters of notification of the proposal were sent to properties in Ives Street and Draycott Avenue, to date, no letters of representation have been received

5.0 **RECOMMENDATION**

5.1 GRANT planning permission

M.J. FRENCH EXECUTIVE DIRECTOR, PLANNING AND CONSERVATION

List of Background Papers:

The contents of file PP/99/01716 save for exempt or confidential information in accordance with the Local Government (Access to Information) Act 1985.

Report Prepared By:

AJF

intros

Report Approved By:

AJF/LAWJ

Date Report Approved:

05/01/2000